

# EXISTING CONDITIONS



## Long Beach Boulevard Midtown



Long Beach Boulevard is one of the few streets in Southern California that truly carries all modes of travel.



Traveling southbound from the off-ramp at Long Beach Boulevard requires a cautious left turn across northbound traffic, which includes cars, buses, and trucks.



The Blue Line provides excellent regional transit access, but it also creates east-west barriers and adds over 20 feet to an already wide roadway with its exclusive travel lanes.



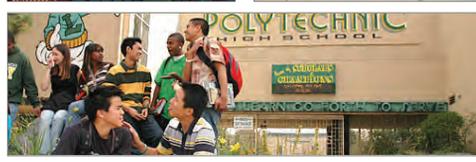
The corridor contains a wide variety of single- and multifamily housing as well as commercial and service businesses.



4th Street Park and Veterans Park



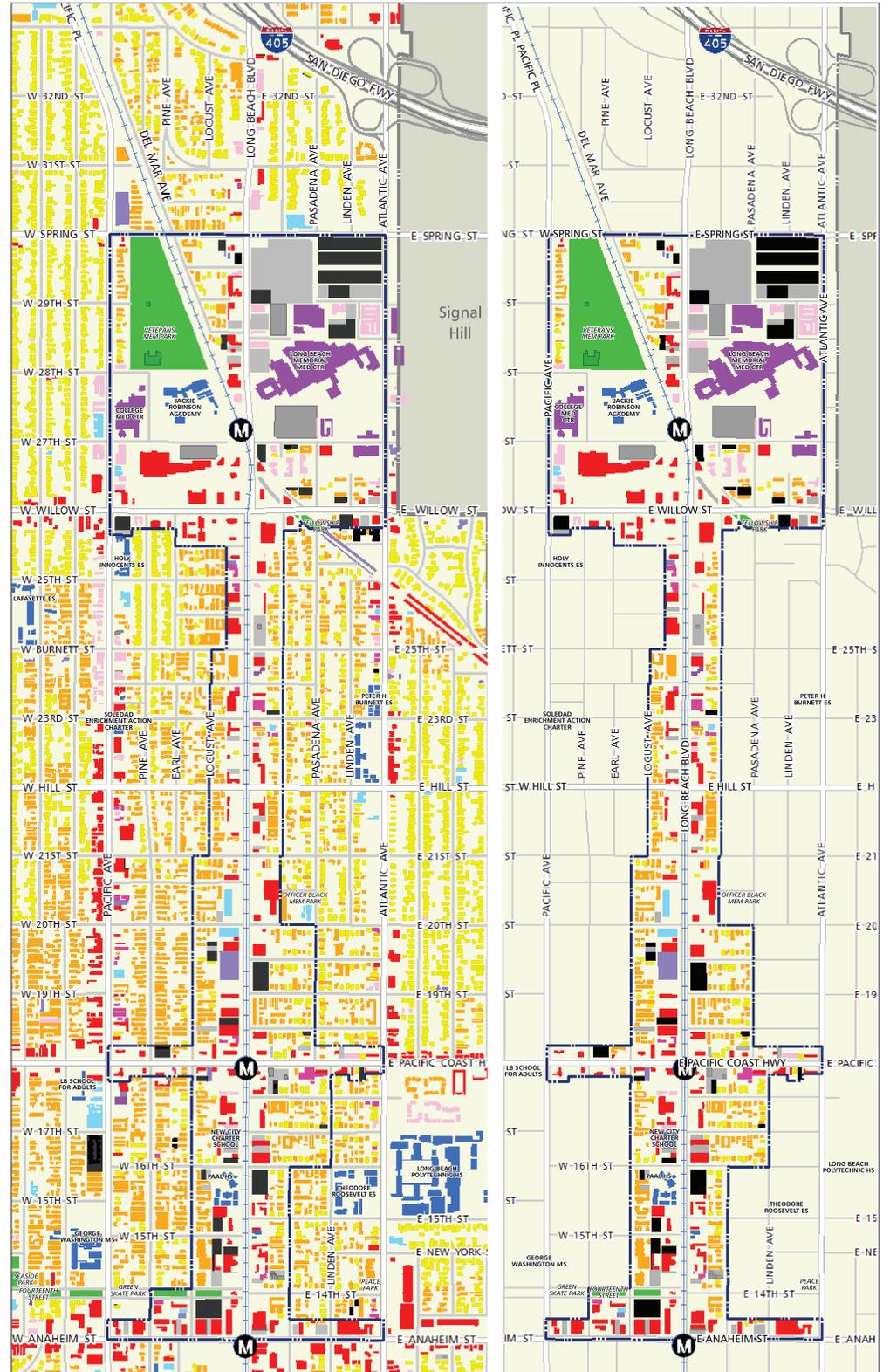
METRO Blue Line Station at Long Beach Boulevard and PCH



From top left, clockwise: New City School, New City Farm, Jackie Robinson Academy, Polytechnic High, Roosevelt Elementary, and Burnett Elementary



Medical facilities also play an important role in Midtown.



### LAND USE PROJECTIONS

	Dwelling Units	Population	Commercial/ Employment Sq Ft	Hospital Beds	Hotel Rooms	Employees
Existing Land Use	1,819	5,695	2,427,567	956	196	12,570
Development Projected Under Proposed Midtown Specific Plan	3,619	10,066	2,776,499	983	277	15,357
Development Levels Allowed Under Current Zoning	5,696	16,528	4,812,965	983	277	20,180

# VISION, PRINCIPLES, AND LAND USE



## Long Beach Boulevard Midtown

### MIDTOWN VISION

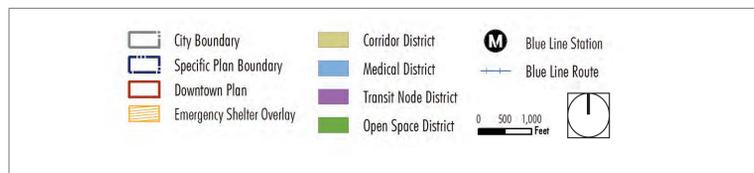
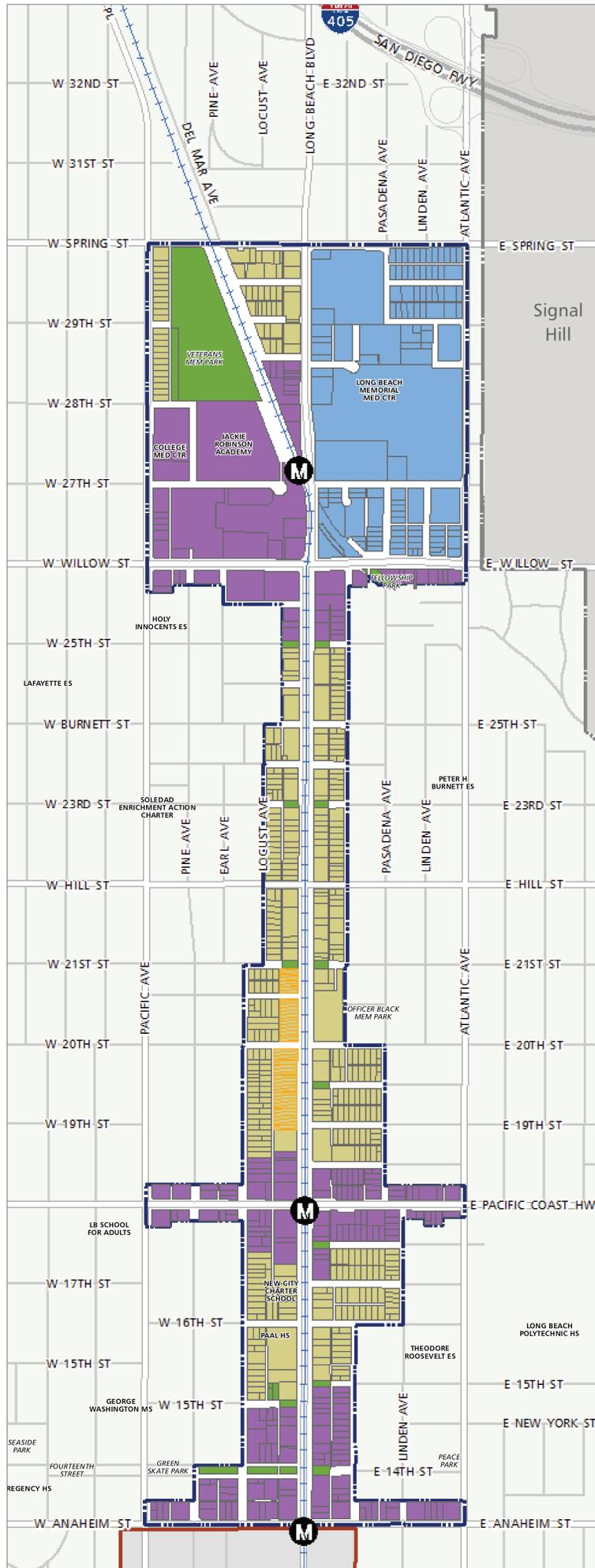
Midtown will be a vibrant and thriving community for our children, family, and friends.

Midtown will be known for its unique blend of parks, strong businesses, and transit-oriented housing.

Additionally, this part of Midtown will be an early leader in multi-modal transportation practices where a person can safely and easily travel by walking, riding a bike, catching a bus, taking a train, or driving a car.

### GUIDING PRINCIPLES

- Enhanced Mobility and Complete Streets
- Safety and Wellness
- A Sustainable Future
- Supporting Infrastructure and Amenities
- Working with and for the Community

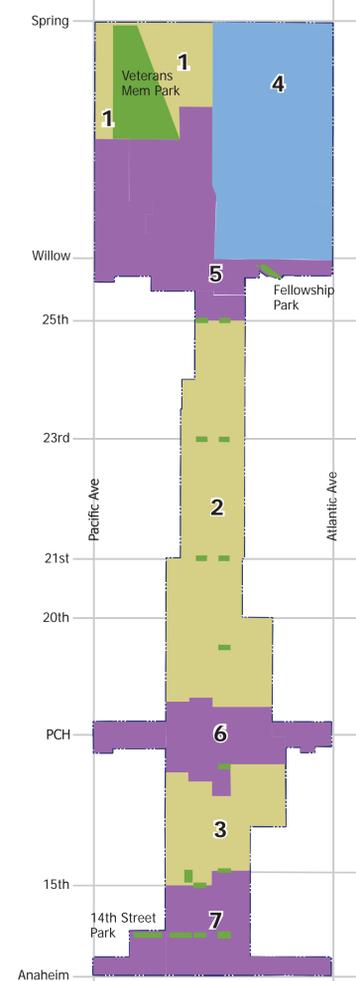


### LAND USE SUMMARY BY DISTRICT

District	Acres	Typical Density (per ac)	Dwelling Units	Comm/Employ Sq Ft	Hotel Rooms/Hospital Beds
<b>Corridor Districts</b>					
1	12	15-40	258	54,000	---
2	51	15-40	924	331,815	---
3	20	15-40	450	92,663	---
<b>Total</b>	<b>83</b>	-	<b>1,632</b>	<b>478,478</b>	---
<b>Medical District</b>					
4	63	20-30	300	757,600	854 beds
<b>Total</b>	<b>63</b>	-	<b>300</b>	<b>757,600</b>	<b>854 beds</b>
<b>Transit Node Districts</b>					
5	44	30-60	1,626	924,296	175 rooms/148 beds
6	20	30-60	724	297,125	102 rooms
7	19	30-60	802	319,000	---
<b>Total</b>	<b>83</b>	-	<b>1,687</b>	<b>1,540,421</b>	<b>277 rooms/148 beds</b>
OS <sup>1</sup>	18	-	-	-	-
ROW	106	-	-	-	-
<b>Total</b>	<b>353</b>	-	<b>3,619</b>	<b>2,776,499</b>	<b>277 rooms/983 beds</b>

Note:  
1. The Open Space District consists of 15.5 acres of existing park area plus 2.6 acres of future parklets. Figures above subject to rounding.

### DISTRICT SUBAREAS



This map divides the land use districts into subareas to summarize the approximate distribution of development potential throughout the Midtown Specific Plan.

# DEVELOPMENT STANDARDS



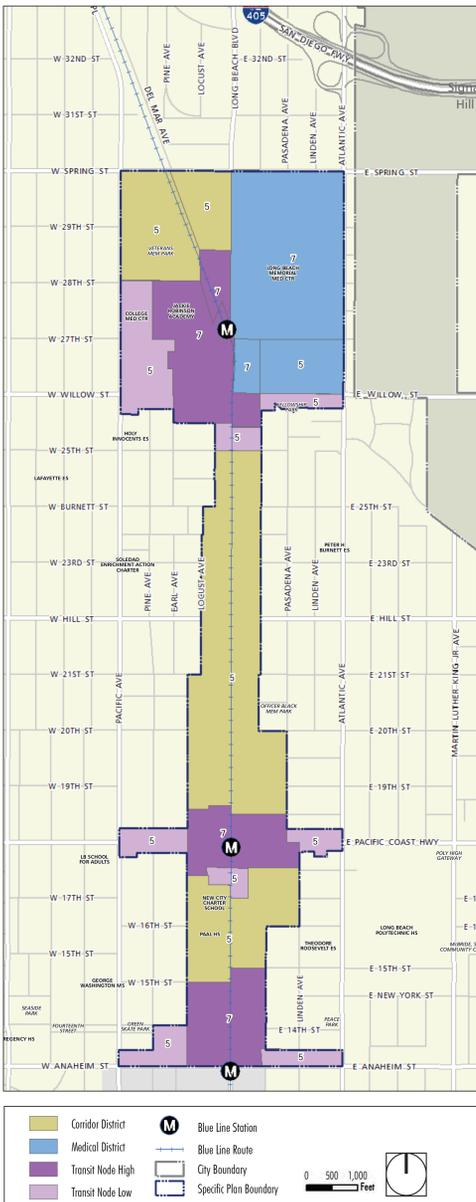
## Long Beach Boulevard Midtown

### DEVELOPMENT INTENSITY STANDARDS

Standard	Transit Node High	Transit Node Low	Corridor	Medical
Maximum building height <sup>1,2,3</sup>				
On parcels <200 feet deep	4 st / 50 ft	3 st / 36 ft	3 st / 36 ft	No Limit
On parcels ≥200 feet deep	10 St / 100 ft	5 st / 65 ft	5 st / 65 ft	
Minimum streetwall height	See Figure 3-3			
Minimum ground floor height	18 ft	18 ft	14 ft	14 ft
Maximum FAR <sup>3,4</sup>				
On parcels <200 feet deep	2.0	1.5	1.5	4.0
On parcels ≥200 feet deep	4.0	3.0	3.0	
Minimum unit size <sup>5,6</sup>	600 sf			
Minimum lot size	10,000 sf			none

#### Notes:

1. Architectural projections are building elements (e.g., towers, cupolas) that are added to building faces to provide architectural interest without adding interior floor area. The maximum height of any architectural projection is 10 feet above the maximum building height.
2. If a project straddles two or more height areas, each height area shall remain in effect, as identified on Figure 3-4, unless approved by the Site Plan Review Committee.
3. Parcel depth shall be measured from the property line parallel to and/or fronting Long Beach Boulevard, Spring Street, Willow Street, Pacific Coast Highway, or Anaheim Street. If a parcel cannot be consolidated with an adjacent parcel (e.g., adjacent parcels are outside of the specific plan or adjacent parcels have already been developed under the specific plan), exceptions can be made by the Site Plan Review Committee.
4. Sections 21.15.1070 and 21.15.1090 of the Municipal Code define and describe FAR.
5. Up to 15 percent of a project's units may be a minimum of 450 sq ft if approved through the Site Plan Review process and if the Site Plan Review Committee finds that the reduced-size units are high-quality dwelling units with sufficient amenities to be livable, desirable dwelling units, to be determined at the sole discretion of the Site Plan Review Committee. A variety of housing unit types and sizes is required for all development projects.
6. Replacement of any unit demolished, as defined in Section 21.15.750 of the Municipal Code, shall be subject to the required new unit size.



3-story mixed-use: residential or office above retail



3-story SF/townhomes or live/work units



3- and 5-story multi-family above retail

### MINIMUM OFF-STREET PARKING REQUIRED

Use	Corridor & Medical	Transit Node	Notes
<b>Residential</b>			
0-1 bedroom	1.0	1.0	per unit
2 bedrooms	1.25	1.25	per unit
3 or more bedrooms	1.5	1.25	per unit
Special group residents, assisted living, congregate care	1.0	0.75	per 3 bedrooms
<b>Senior housing</b>			
Market rate/rent	1	0.75	per bedroom
Income restricted/low rent	.05	0.33	per bedroom
Shopkeeper or live-work	1.5	1.25	per unit
Guest parking	1.0	1.0	per 4 units
<b>Non-residential</b>			
Hotel	0.5	0.5	per room
Medical office	5	3	per 1,000 sq ft
Hospital	2	2	per bed
All other uses	2.0	2.0	per 1,000 sq ft

In the Transit Node District, this requirement only applies to non-residential building space in excess of 4,000 sq ft. Restaurants calculated based on sq ft of dining area; no additional parking requirement for the first 250 sq ft of outdoor dining space.

The Midtown Specific Plan is also known as Planned Development District 29 (PD-29) and regulates the project area through four development districts: Transit Node, Corridor, Medical, and Open Space. Each district would be governed by development standards and land use patterns. Overall, development would be concentrated at key transit and employment nodes.



Multi-story entertainment or hotel



Medical campus



Medical campus

### BICYCLE PARKING

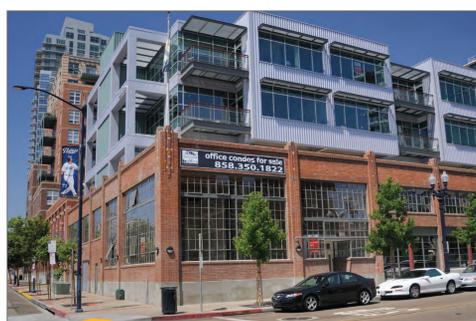
Use	Minimum Bicycle Capacity	Type of Parking Facility	Location
Residential, shopkeeper unit, or live-work unit	1.0 space per 5 units, 1 enclosed locker required for every 50 dwelling units	A-frame or freestanding rack	Near main entrance with good visibility, not to obstruct auto or pedestrian movement
Commercial	1.0 space per 5,000 sq. ft. of building area	Staple or new technology	
Retail	1.0 space for each 7,500 sq. ft. of building area	Staple or new technology	
Schools	8.0 spaces per 40 students	A-frame, freestanding racks	Near office entrance with good visibility, in fenced area
Public facilities	8.0 spaces per location	Staple or freestanding racks	Near office entrance with good visibility
Transit stations	1.0 space per 30 parking spaces	Lockers	Near platform or security guard



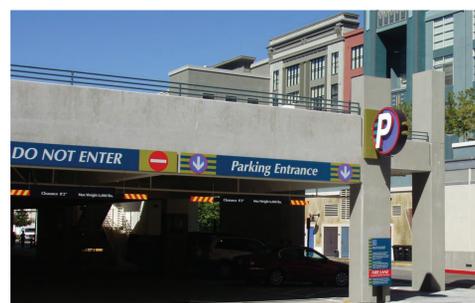
5-story mixed-use: residential or office above retail



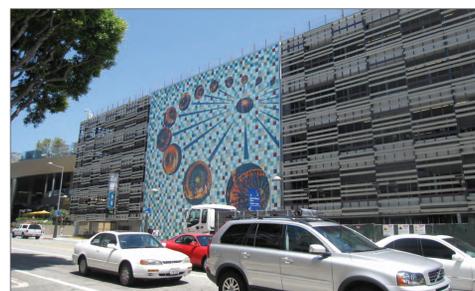
5-story mixed-use: residential or office above retail



6-story mixed-use: residential or office above retail



Structured parking



A parking garage as a public art opportunity



Bicycle parking



Stacked car or bike parking



4-story residential

# DRAFT STREET SECTIONS AND COMPLETE STREETS



## Long Beach Boulevard Midtown

**BOULEVARD TYPICAL MIDBLOCK STREET SECTION (MULTI-MODAL WITH BIKE PATH)**

**General Plan Mobility:** Primary Transit & Pedestrian Priority Street  
**Mobility Supported:** Bus and Rail Transit, Pedestrian, Bike, Vehicle

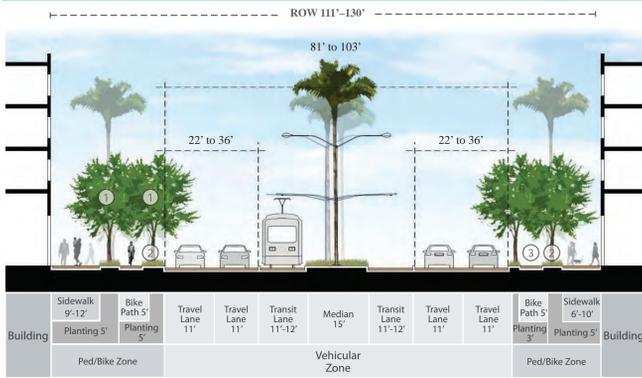
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Transit amenities
- Transit only and shared transit lanes
- New bicycles lanes
- Signal synchronization

**Streets**  
 Long Beach Boulevard between Willow Street & Anaheim Street

**Section Notes**

- Additional canopy trees
- Landscaping buffer zone
- Enhanced separated bike lane at curb level



**BOULEVARD TYPICAL MIDBLOCK STREET SECTION (WITHOUT BIKE PATHS)**

**General Plan Mobility:** Secondary Transit Priority Street  
 Pedestrian Priority Area

**Mobility Supported:** Pedestrian, Vehicle

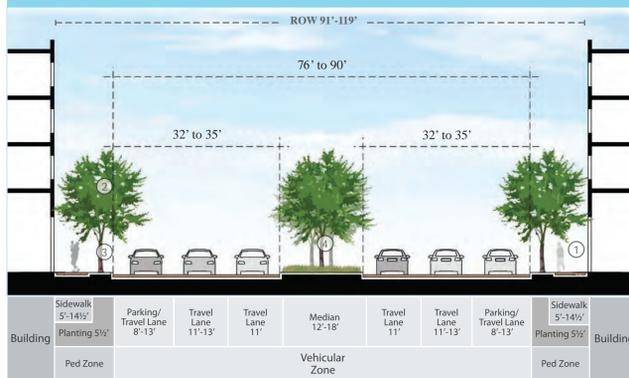
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Transit amenities
- Signal synchronization

**Streets**  
 Willow Street  
 Long Beach Boulevard between Spring Street & 27th Street

**Section Notes**

- Wider sidewalks
- Additional canopy trees
- Landscaping buffer zone
- Planted center median



**REGIONAL CORRIDOR TYPICAL MIDBLOCK STREET SECTION**

**General Plan Mobility:** Transit & Pedestrian Priority Street

**Mobility Supported:** Bus and Rail Transit, Pedestrian, Bike, Vehicle

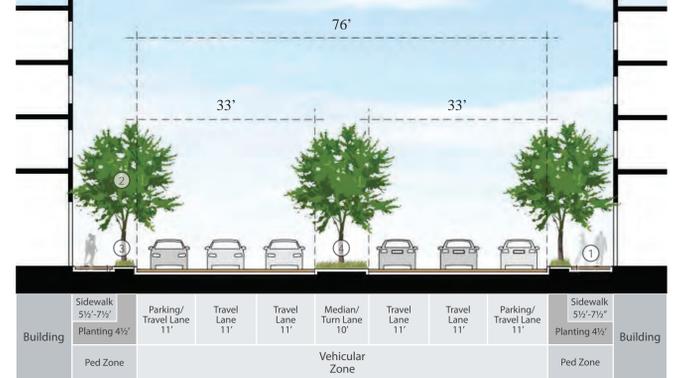
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Transit amenities
- Transit only and shared transit lanes
- Signal synchronization

**Streets**  
 Pacific Coast Highway

**Section Notes**

- Wider sidewalks
- Additional canopy trees
- Landscaping buffer zone
- Planted center median



**MAJOR AVENUE TYPICAL MIDBLOCK STREET SECTION (WITH BIKE LANES)**

**General Plan Mobility:** Varies

**Mobility Supported:** Bus, Pedestrian, Bike, Vehicle

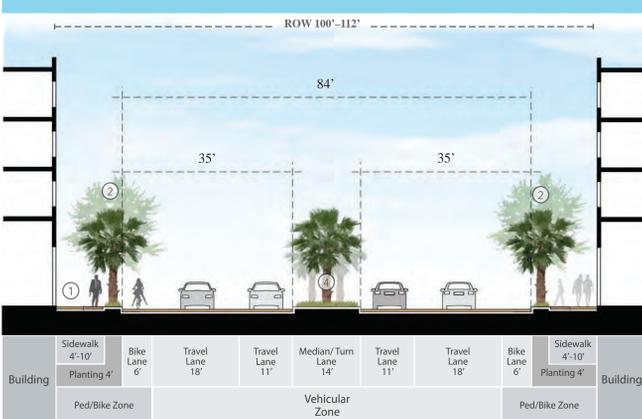
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Shade for sidewalks & bicycle lanes
- Incorporation of planting areas along curb

**Section Notes**

- Wider sidewalks
- Additional canopy trees
- Landscaping buffer zone
- Planted center median

**Streets**  
 Spring Street between Long Beach Boulevard & Atlantic Avenue



**MAJOR AVENUE TYPICAL MIDBLOCK STREET SECTION (WITHOUT BIKE LANE)**

**General Plan Mobility:** Varies

**Mobility Supported:** Bus, Pedestrian, Vehicle

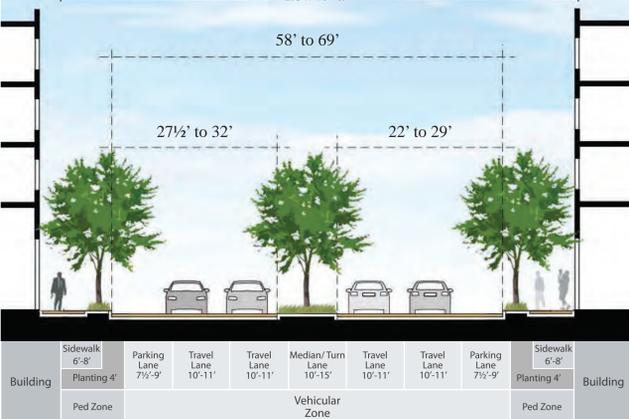
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Bicycle Lanes
- Signal synchronization

**Section Notes**

- Wider sidewalks
- Additional canopy trees
- Landscaping buffer zone
- Planted center median

**Streets**  
 Atlantic Avenue  
 Anaheim Street



**MINOR AVENUE TYPICAL MIDBLOCK STREET SECTION**

**General Plan Mobility:** Not a Priority Street

**Mobility Supported:** Bus, Pedestrian, Vehicle

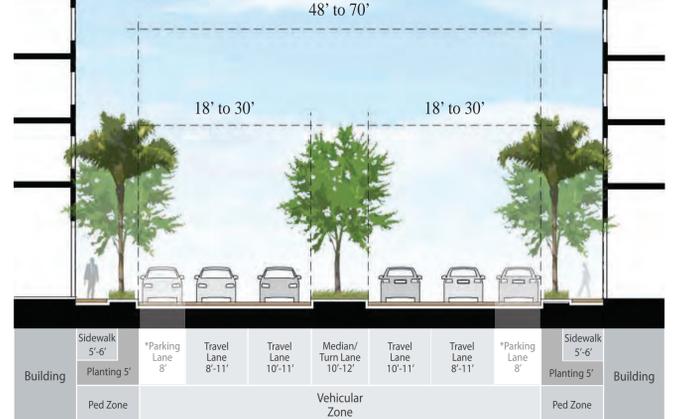
**Key Features and Enhancements**

- A focus on the pedestrian experience
- Signal synchronization

**Section Notes**

- Additional canopy trees
- Landscaping buffer zone
- Planted center median

**Streets**  
 Pacific Avenue  
 Spring Street between Pacific Avenue & Long Beach Boulevard



**NEIGHBORHOOD CONNECTOR AND LOCAL STREET TYPICAL MIDBLOCK STREET SECTION**

**General Plan Mobility:** Not a Priority Street

**Mobility Supported:** Pedestrian, Bike, Vehicle

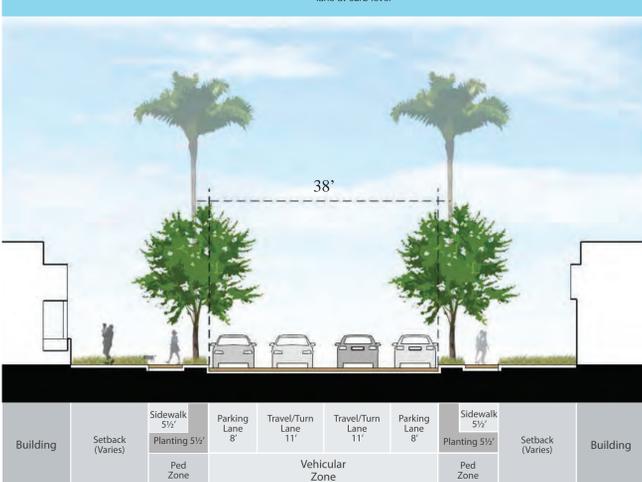
**Key Features and Enhancements**

- A focus on the pedestrian experience

**Section Notes**

- Wider sidewalks
- Additional canopy trees
- Landscaping buffer zone
- Enhanced separated bike lane at curb level

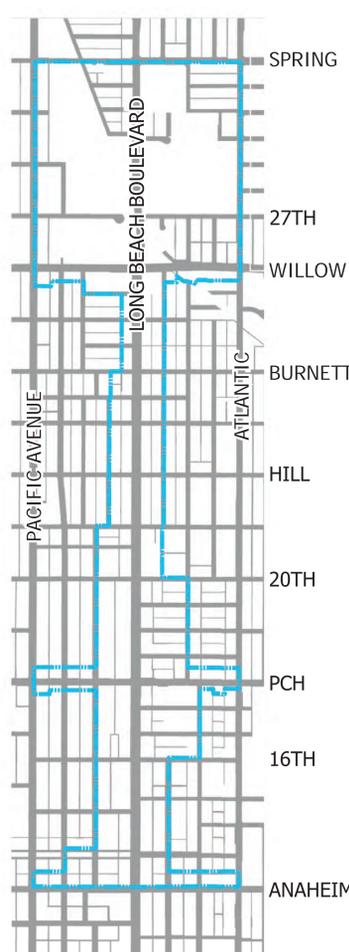
**Streets**  
 Streets not otherwise noted



### COMPLETE STREETS

A complete streets roadway network provides safe and convenient access for all users—motorists, bicyclists, pedestrians, and transit riders. Complete streets are accessible to all ages and abilities. They are designed and operated to make it easier to cross the street, walk to shops, and bicycle to work. Ultimately, they improve safety for all users.

The draft street designs for the Midtown Specific Plan area combine existing amenities with new features, such as: additional bike lanes, wider sidewalks, landscaping buffers, and improved intersection crossings. This corridor benefits from access to the Metro Blue Line and a potential future connection to the Green Line. Special care has been taken to improve access to the Metro stops for multiple modes of transportation.



**SAMPLE MIDBLOCK STREET SECTION**

**General Plan Mobility:** Secondary Transit Priority Street

**Mobility Supported:** Pedestrian, Vehicle, Bicycle, Transit

**Key Features and Enhancements**

- A focus on the pedestrian experience
- Transit amenities
- Signal synchronization

**Streets**  
 Long Beach Boulevard

**Section Notes**

- Additional canopy

Icons indicating multi-modal transportation options

List of the streets this section applies to

Provides call outs for enhanced features of the streetscape, such as additional canopy trees

Section note

Travel zones and applicable widths

ROW XXX'-XXX'

CURB-TO-CURB

TRAVEL LANES

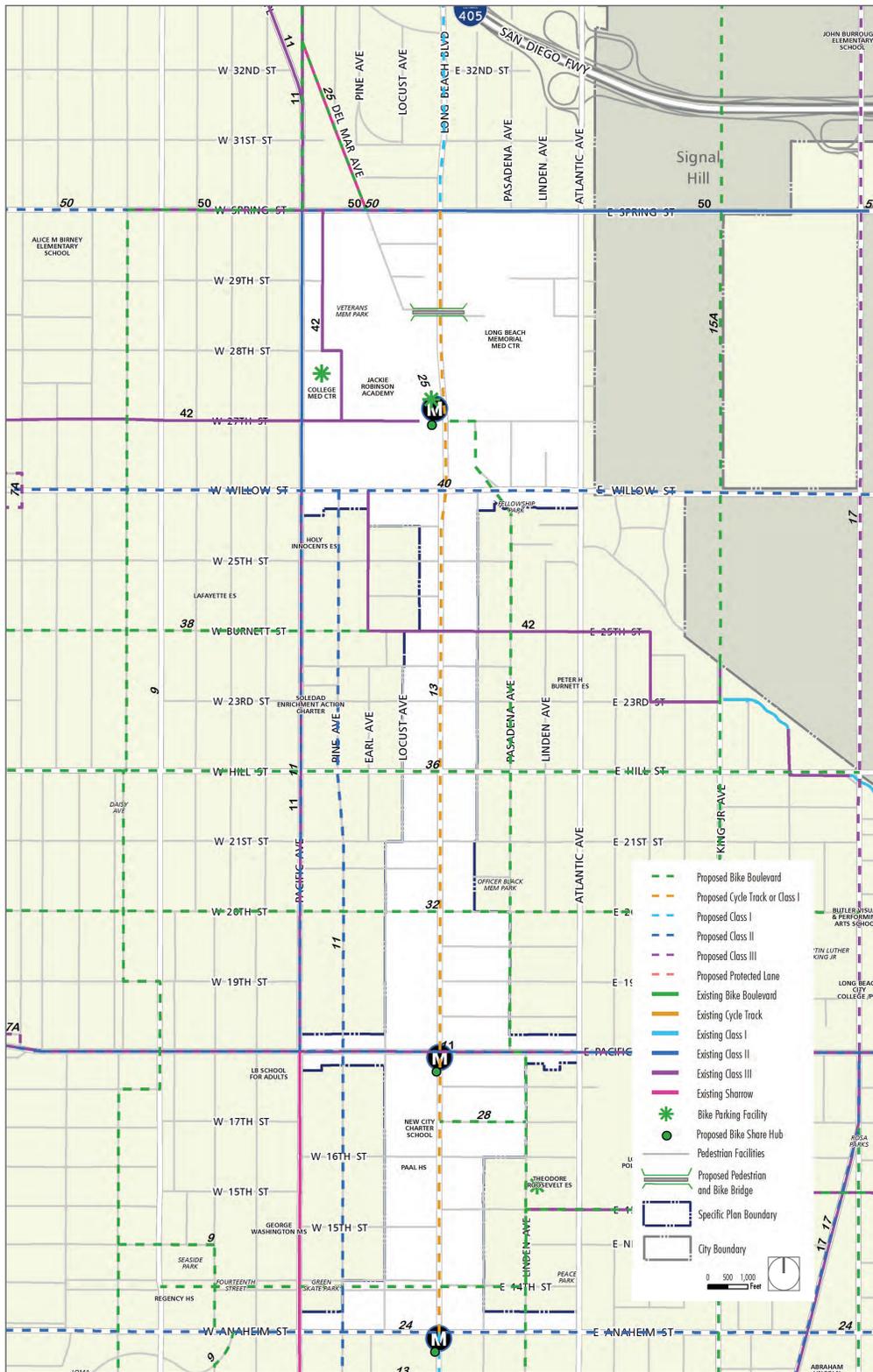
TRAVEL LANES

Building	Sidewalk	Bike Path	Parking/Travel Lane	Travel Lane	Transit Lane	Median	Transit Lane	Travel Lane	Parking/Travel Lane	Bike Path	Sidewalk	Building
	Ped/Bike Zone		Vehicular Zone					Ped/Bike Zone				

# BIKEWAY AND PEDESTRIAN IMPROVEMENTS



## Long Beach Boulevard Midtown



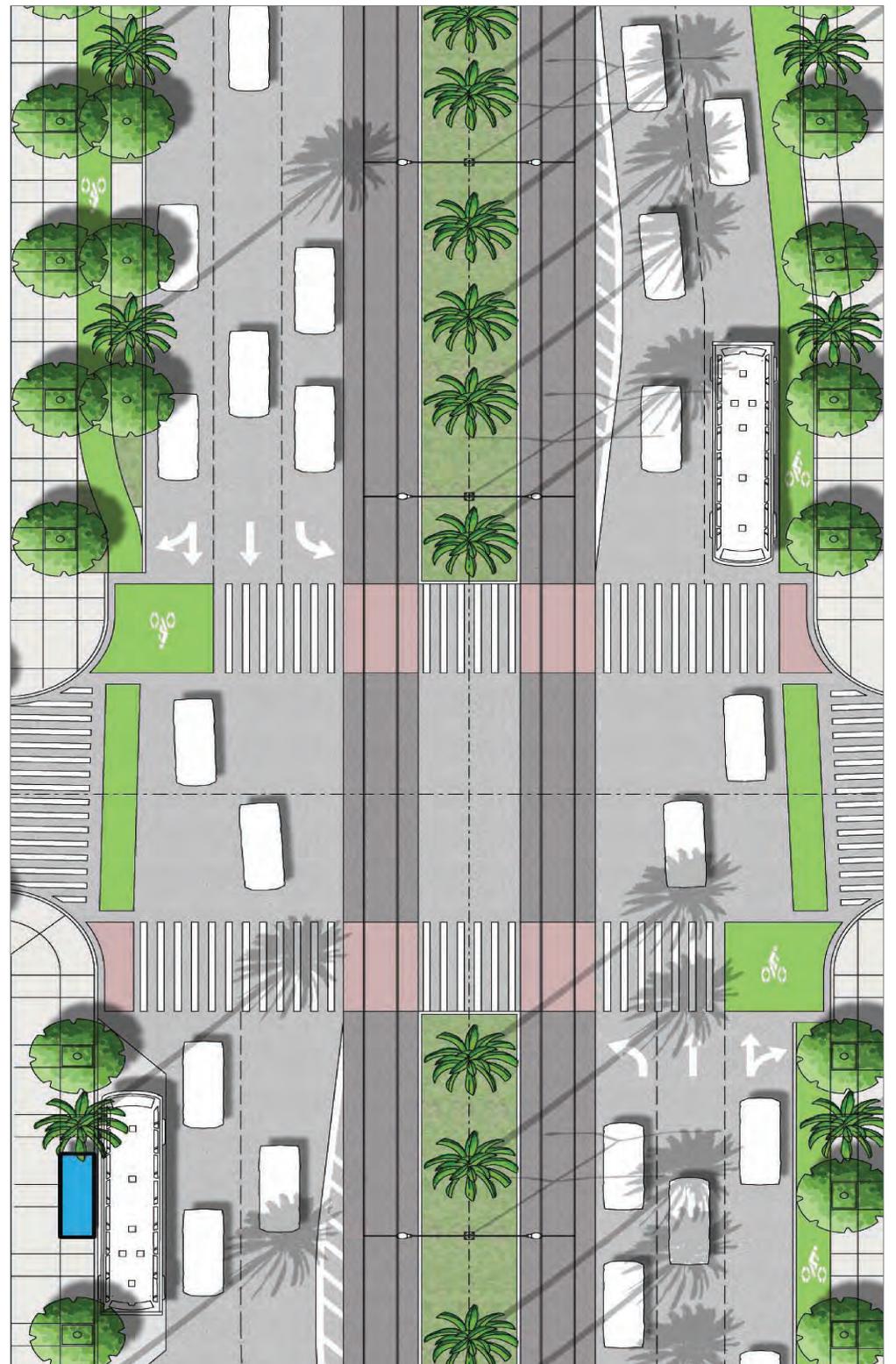
### PROPOSED IMPROVEMENTS

The City is considering changes to enhance mobility and improve Long Beach Boulevard as a complete street, a roadway network that provides safe and convenient access for all users—pedestrian, bicycle, transit, and automobile.

This Plan could introduce new bikeways and bike boxes along Long Beach Boulevard. New bike lanes could be physically separated from pedestrian and vehicular traffic.

Additionally, this Specific Plan could require the inclusion of new bike lockers and racks throughout Midtown, with the largest concentration in Transit Node Districts and at Metro stations.

Pedestrians would benefit from many of the bicycle improvements with some additional feet-friendly options. The creation of separated bike lanes would improve safety, and widening the sidewalk would increase usability. The addition of canopy trees would provide much-needed shade and add color to the public realm. Other enhancements include parklets that could serve as oases amid the corridor and a potential bike and pedestrian bridge linking the Medical Center, Veterans Park, and Willow Transit Station.



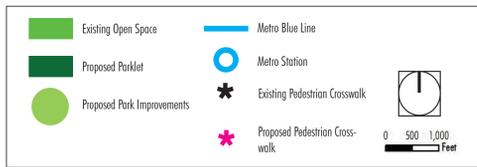
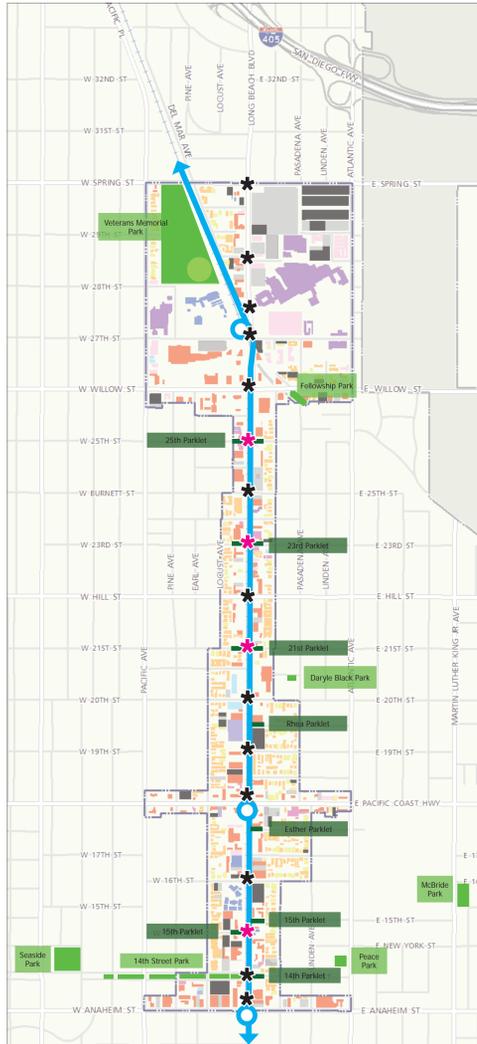
DRAFT ILLUSTRATIONS ARE PROVIDED FOR CONCEPTUAL PURPOSES.



# PARKS AND PARKLETS



## Long Beach Boulevard Midtown



The City is exploring opportunities for additional open space in Midtown through enhanced public park space, improved public rights-of-way, and private on-site space.

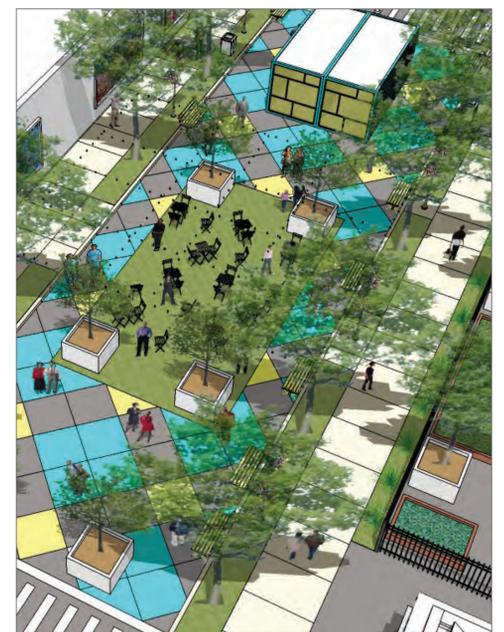
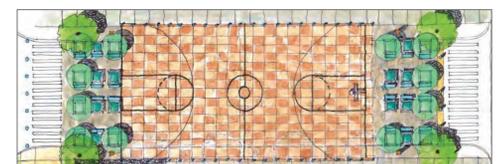
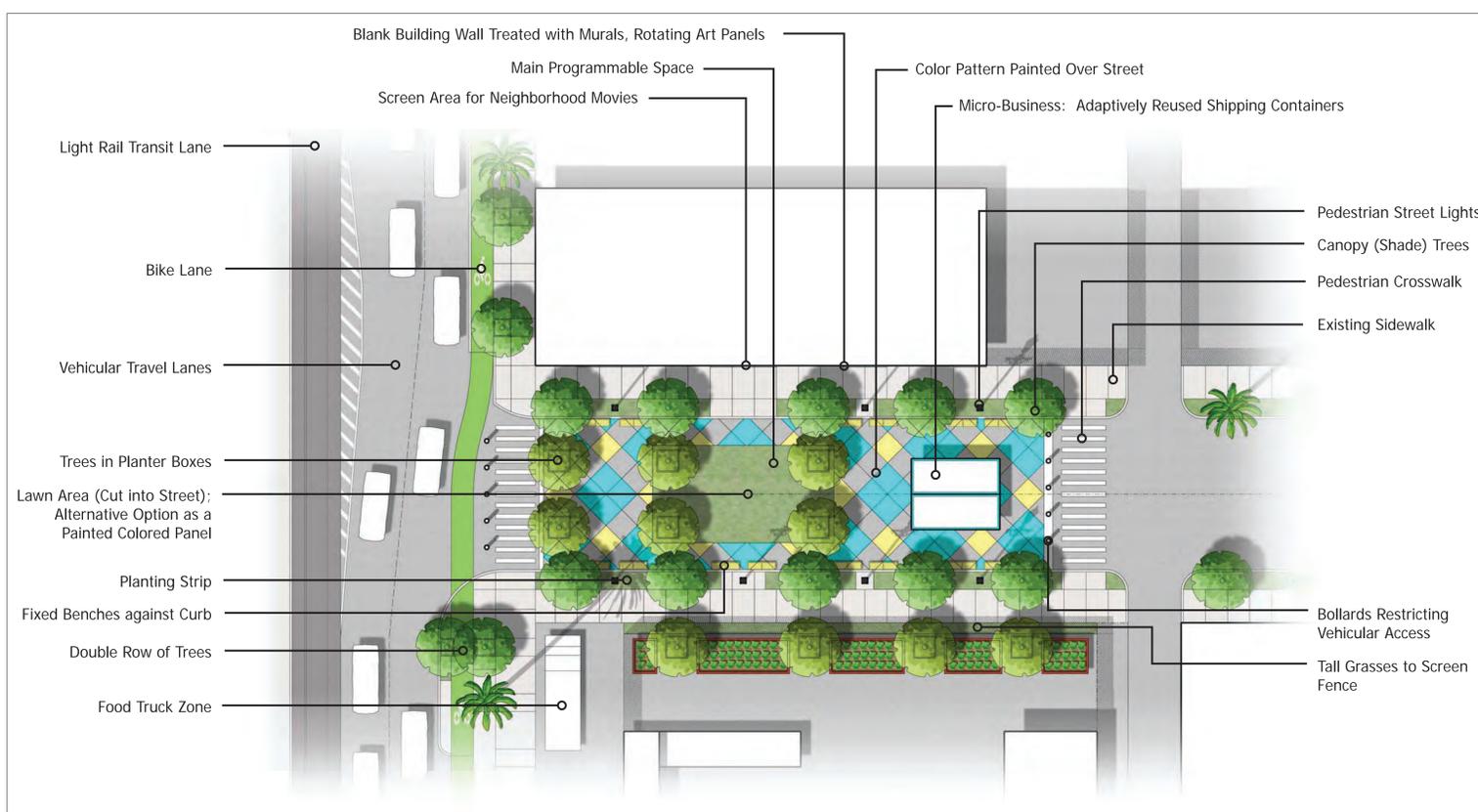
This includes new parklets—small parks about a quarter acre in size placed on reclaimed street right-of-way.

Private on-site open space could include private or public spaces including courtyards, green or active roofs, and/or other gathering areas.

### PRIVATE OPEN SPACE REQUIREMENTS

Use	On-Site Open Space Requirement	Minimum Dimensions
Residential	50 sq ft per unit	5 ft
Non-residential	10% of the project area	10 ft

Notes:  
 1. Mixed-use projects are subject to the requirements of this table in an additive manner, residential and non-residential requirements apply to the proportionate area of each use within the project.  
 2. All requirements apply to attached, private open space.



DRAFT ILLUSTRATIONS ARE PROVIDED FOR CONCEPTUAL PURPOSES.

# ENVIRONMENTAL PROJECT BOUNDARY



## Long Beach Boulevard Midtown

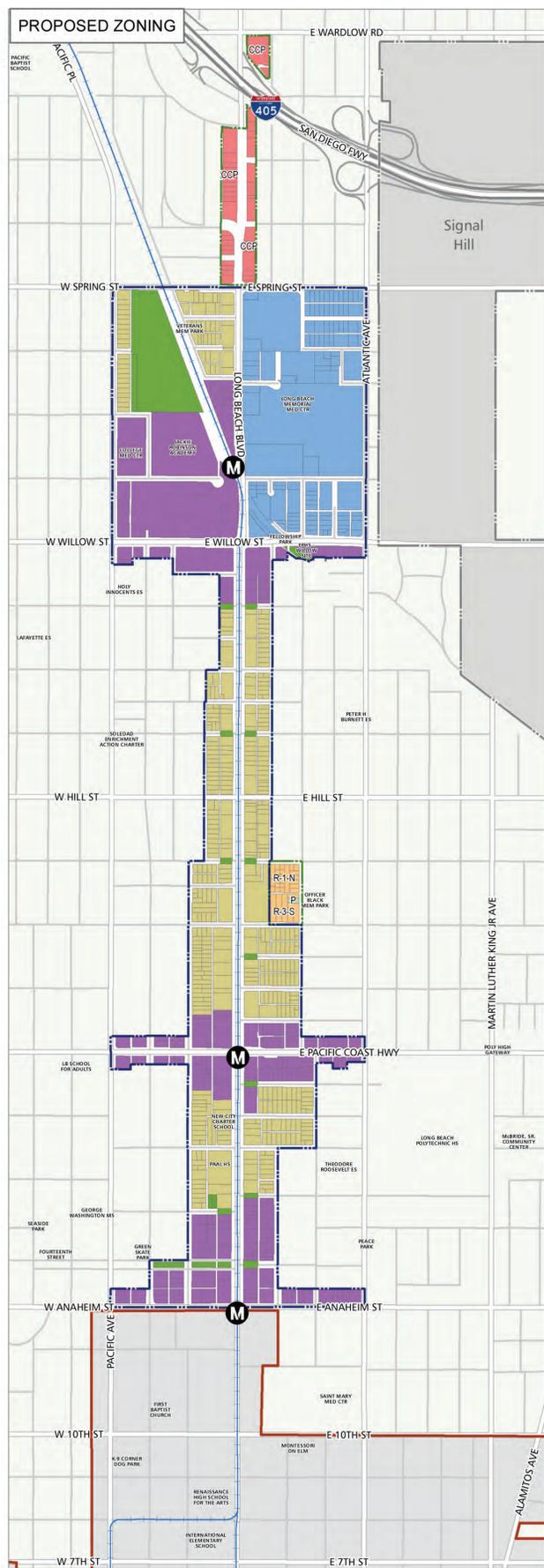


- M** Blue Line Station
- Blue Line Route
- PD-29 Sphere
- PD-29 Boundary
- Downtown Plan Boundary
- City Boundary
- R-2-N Zone
- R-4-R Zone
- CCA/CHW/CH/CNA/CNP/CCN
- PD-22/PD-25
- I - Institutional
- P - Park
- PR - Public ROW

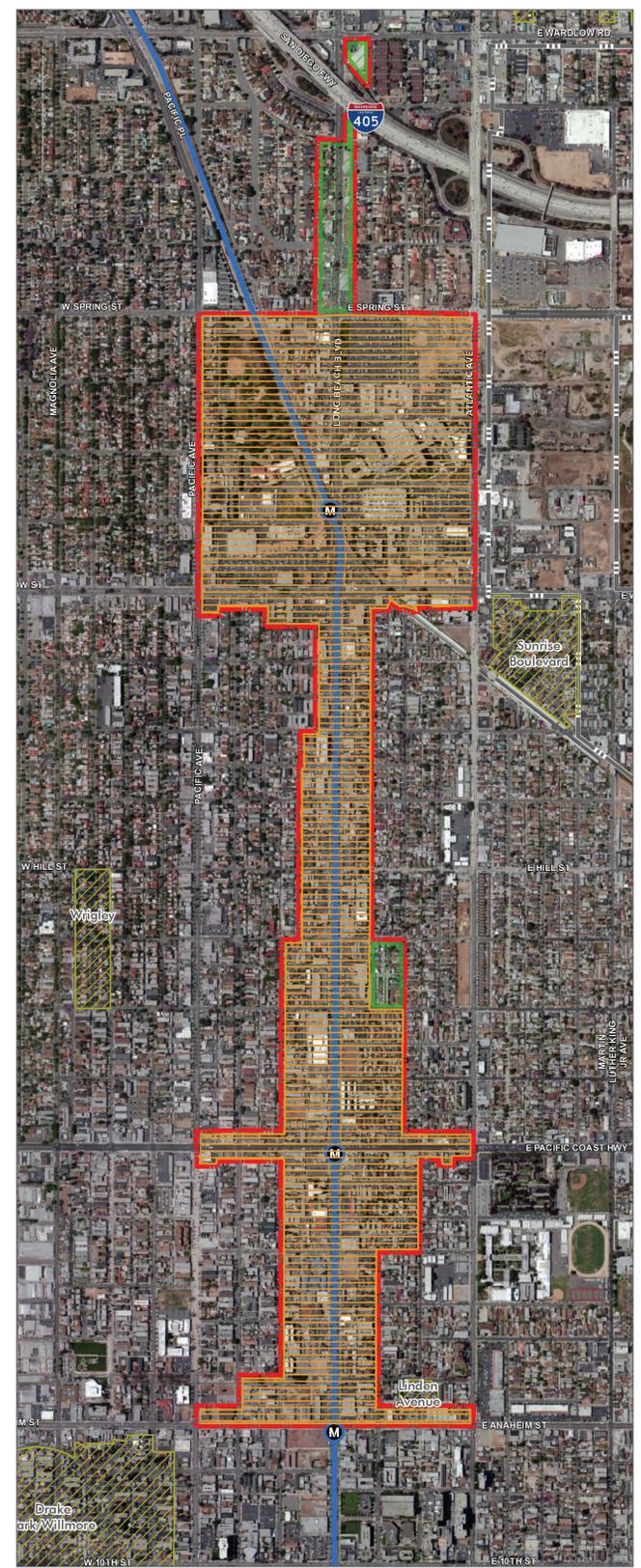
### PROJECT LOCATION

The Project Site (generally situated east of Pacific Avenue, west of Atlantic Avenue, north of Anaheim Street, and south of Wardlow Road) is just north of downtown Long Beach and consists of three areas: the Midtown Specific Plan area and two Conventional Zoning areas.

The Midtown Specific Plan area spans approximately 353 acres from Anaheim Street to Spring Street. The first Conventional Zoning area covers 15 acres from Spring Street



- M** Blue Line Station
- Blue Line Route
- Specific Plan Boundary (PD-29)
- Conventional Zoning Boundary (formerly PD-29)
- Downtown Plan Boundary
- City Boundary
- Corridor District
- Medical District
- Transit Node District
- Open Space District
- CCP Zone
- R-1-N Zone
- R-3-S Zone
- P Zone



- Project Site Boundary
- Conventional Zoning Area
- Midtown Specific Plan Site Boundary
- Historic Districts
- City Boundary
- Blue Line Station
- Blue Line Route

**Table 1 Land Use Projections for Midtown Specific Plan Area**

	Dwelling Units	Population	Com/Emp Square Feet	Hospital Beds	Hotel Rooms	Employees
Existing Land Use	1,819	5,695	2,427,567	956	196	12,570
Development Projected Under Proposed Midtown Specific Plan	3,619	10,066	2,776,499	983	277	15,357
Development Levels Allowed Under Current Zoning	5,696	16,528	4,812,965	983	277	20,180

Notes: Com/Emp = Commercial/Employment

**Table 2 Land Use Projections for Conventional Zoning Area**

	Dwelling Units	Population	Com/Emp Square Feet	Hospital Beds	Hotel Rooms	Employees
Existing Land Use	140	438	212,112	0	0	241
Development Projected Under Proposed Zoning	76	246	237,852	0	0	538
Development Levels Allowed Under Current Zoning	247	773	192,362	0	0	429

**Table 3 Overall Land Use Projections for Proposed Project**

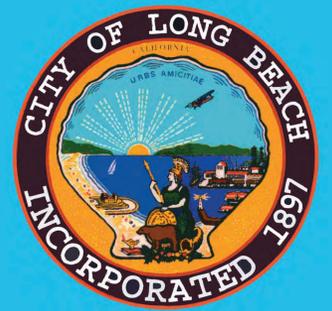
	Dwelling Units	Population	Com/Emp Square Feet	Hospital Beds	Hotel Rooms	Employees
Existing Land Use	1,959	6,133	2,639,679	956	196	12,811
Development Projected Under Proposed Project	3,695	10,312	3,014,351	983	277	15,895
Development Levels Allowed Under Current Zoning	5,943	17,301	5,005,327	983	277	20,609

Notes: Com = commercial, Emp = employment

to Wardlow Road, and the second area covers 5 acres near Officer Black Park.

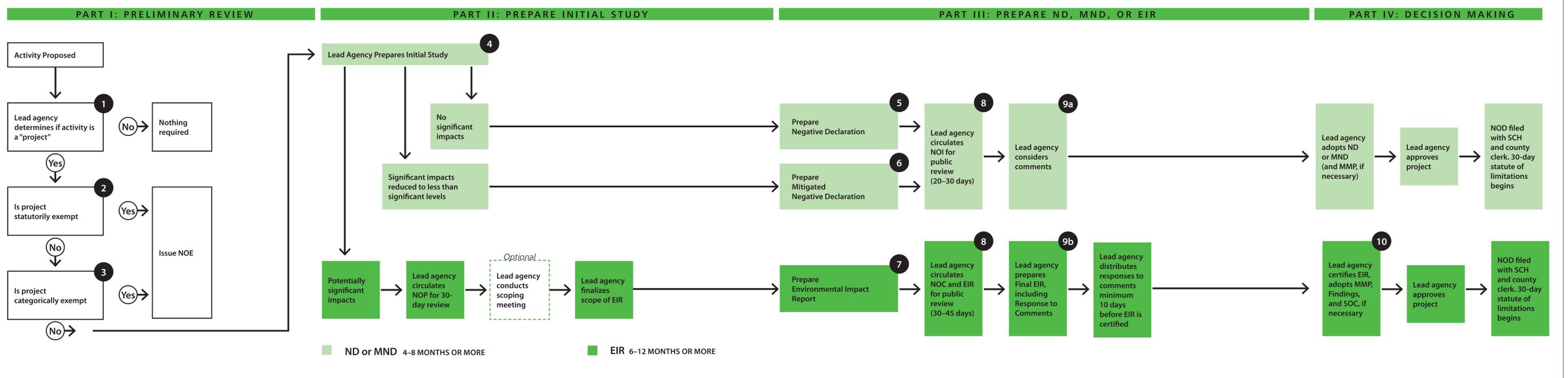
All three areas make up the Project Site and together, comprise 373 acres spanning from Anaheim Street to Wardlow Road. The eastern and western boundaries of the Project Site range from 300 feet at midblock locations to quarter-mile transit nodes along California State Route 1 (SR-1) and Anaheim Street.

# ENVIRONMENTAL IMPACT REPORT

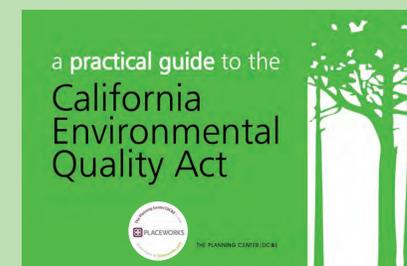


Long Beach Boulevard Midtown

## The CEQA Process in a Nutshell



- 1 Determining if the Action is a "Project"
- 2 Statutory Exemptions
- 3 Categorical Exemptions
- 4 Initial Study
- 5 Negative Declaration
- 6 Mitigated Negative Declaration
- 7 Environmental Impact Report
- 8 Public Review
- 9a Response to Comments (ND/MND)
- 9b Response to Comments and Final EIR
- 10 Certification and Other Decisions



This process chart is excerpted from *A Practical Guide to the California Environmental Quality Act*. This publication provides a basic outline of the CEQA process, with simple definitions and tips from experts to clarify complex issues.

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